



City of
Summit

Sidewalk
Installation &
Maintenance
Plan

Dept. of Community Services
Division of Engineering
9/19/2019

Aaron Schrager, City Engineer
Rick Matias, Assistant City Engineer
Lori Toth, Assistant Engineer

Approved by Council on:
October 15, 2019

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Intersection of Springfield Avenue and Woodland Avenue



PURPOSE

The City of Summit's *Sidewalk Installation & Maintenance Plan* sets forth the respective procedures and standards for public sidewalks and pedestrian pathways throughout the City. This document is intended to direct the City in its prioritization, budgeting and standardization in constructing new sidewalk and enhancing existing sidewalk facilities, along with private and public maintenance of sidewalks and sidewalk networks.

Safe pedestrian walkways are a vital component of the community's established family-oriented character and contribute to its strong sense of place and high quality of life for those who live, work and shop in Summit. They also encourage walking as an alternative mode of transportation. Walking promotes individual health, reduces traffic and carbon emissions and fosters a sense of community by creating opportunities for people to meet and experience the City. Such interactions are enhanced through aesthetic and functional improvements including landscaping, lighting, street furniture and art, among other features, which generate memorable experiences and encourage recurring walks.

The implementation of policies pertaining to sidewalk location, prioritization and capital maintenance, establishes consistency throughout the City which in turn will effectively improve pedestrian responses to various street characteristics and ensure that related safety issues can be relegated. The City primarily focuses on safety when evaluating new sidewalk installations, particularly in areas within walking distance to schools, to amenities that serve seniors, the Central Retail Business District (CRBD), Neighborhood Business Zones, public transportation stops, and other highly used public amenities located throughout the City.

In administering Summit's *Sidewalk Installation & Maintenance Plan*, the City will continue to perform the various tasks outlined herein, which include the collection and evaluation of data, mapping existing and proposed conditions, ensuring conformance with City, State and national standards, continued maintenance of sidewalks, sidewalk networks and ultimately, prioritizing and implementing pedestrian network improvements.

DEFINITIONS

ADA: Americans with Disabilities Act; a set of laws approved by the United States Congress in 1990 prohibiting the discrimination of individuals with disabilities in all areas of public life, including employment, schools, transportation and all public and private places that are open to the general public. Amendments were made to the Act in 2008.

Complete Street: A conceptual design approach to a planned street to enable the safe, convenient and comfortable travel for all users and their respective abilities that is not dependent on their mode of transportation. Typically the design of the road will include access for pedestrians, vehicles, bicycles and mass transportation vehicles.

CRBD: Central Retail Business District – A dedicated planning zone within the City of Summit

Heat Island: Describes built up areas, neighborhoods or zones that are hotter in temperature than other less built up areas within a geographic space.

High Priority Sphere (“Spheres”): Areas within ¼-mile radius of the main access point(s) to the five primary schools, the middle school, Investors Bank Field and Memorial Field complexes, and around the CRBD, including the library, train station, swim clubs, senior housing facilities and other parks or facilities.

MUTCD: Manual on Uniform Traffic Control Devices defines the standards used by road managers nationwide to install and maintain traffic control devices on all public streets, highways, bikeways and private roads open to public travel. The MUTCD is published by the Federal Highway Administration (FHWA) under the Code of Federal Regulations, 23 Part 655, Subpart F.

NJDOT: New Jersey Department of Transportation; a state department concerning itself with transportation within New Jersey.

RRFB: Rectangular Rapid Flashing Beacon is a mounted flashing crosswalk sign that is typically activated manually by a pedestrian attempting to cross a street crossing. The majority of these RRFBs are solar powered.

School Walking Route: Formal routes prepared for each elementary school collectively by the Police Department, Parent Teacher Organizations, Board of Education and City Engineering Division. The routes are used as a tool for students and parents to set sidewalk installation priorities.

Sidewalk: The portion of a roadway intended for the use of pedestrians between the curb or edge of pavement and the adjacent right of way or property line. It is typically constructed of a rigid, impervious and cementitious material that is poured in place using forms, base material and finishing tools.

RGO: Revised General Ordinances of the City of Summit approved by City of Summit Common Council and formally adopted on December 17, 2019. These ordinances are amended from time to time.

DRO: Development Regulations Ordinances of the City of Summit is the zoning chapter of the City's RGO (Chapter 35) pertaining to development within the City of Summit.

EXISTING POLICIES, REGULATIONS AND GUIDES

The following are ordinances, policies and guidelines currently in place within the City.

1. City of Summit RGO; Chapter XVIII, – Streets and Sidewalks

Standard specifications for the installation and maintenance of sidewalks within the City are governed by Chapter 18 of the City's General Ordinance.

Chapter 18 of the City Ordinance includes the following provisions, among others:

18-1.7.a.2:

All sidewalks and driveway aprons shall be concrete.

18-5.1:

All sidewalks shall be maintained and kept by the homeowner or owners of the lots on which the sidewalks front, in good sufficient condition, even on the surface and so graded as freely to shed water. And in all cases in which at any time such sidewalk is not in the condition required by this section such owner or owners shall cause the same to be replaced and to be put in the condition above required.

18-9:

No person, firm or corporation shall remove an existing sidewalk without the permission of the Common Council.

18-11: (*References to the Public Works Committee have been changed to the Capital Projects and Community Services Committee*)

- a. Whenever it is deemed necessary at the request of the P.T.A. Safety Council or the City of Summit staff, concurred in by the Common Council, in the interest of safety of pedestrians to install new sidewalks, the cost shall be encumbered by the City. Each project shall be approved by the Capital Projects and Community Services and Public Safety Committees.

- b. For all streets within the designated school walking routes, sidewalks must be installed continuously from the school property boundary for a distance of one-quarter (1/4) mile on a minimum of one (1) side of the street. The distance shall be measured along the walking route. Once this criterion has been fully satisfied through sidewalk installation, the distance shall be increased to one-half (1/2) mile and three-quarter (3/4) miles respectively.
- c. For all streets adjacent to the CRBD, sidewalks must be installed continuously from the outer limits of the CRBD for a one-quarter (1/4) mile radius on a minimum of one (1) side of the street.
- d. For neighborhoods that do not meet the conditions set forth under paragraphs b. and c. but desire sidewalk, a signed petition of support must be formally submitted that contains a minimum of sixty-six (66%) percent of the residents located on the sidewalk receiving side of the street. If support is obtained, the project would then be considered for the next annual pedestrian safety improvement project.
- e. Sidewalks shall be installed as part of any capital improvement project on streets that meet the requirements of paragraphs b. and c.
- f. For streets that meet the requirements of paragraphs b. and c. and are not scheduled for a capital improvement project, the sidewalk shall be considered for the next annual pedestrian safety project. The projects shall be approved by the Capital Projects and Community Services Committee annually.
- g. Any property owner submitting a site plan to the City shall be required to install sidewalk as required by paragraphs b. and c. if applicable.

2. City of Summit Complete Streets:

City sidewalk and pedestrian improvements shall be installed in accordance with the City's Complete Street Policy adopted by Common Council on June 4, 2014 by Resolution No. 36325.

Resolution No. 36325 includes the following provisions:

- a. All public street projects including new construction and reconstruction shall be designed and constructed using complete streets policies where practical and safe.
- b. Pedestrian facilities shall not be required where prohibited by law or would result in unsafe conditions.
- c. When the cost of incorporating new pedestrian facilities is greater than 20% of the overall project cost based on written estimates and adequate supporting documentation, the need for and use of the facility shall be considered in making the determination as to the budget and capital improvement program approval process or when project plans and specifications are being prepared.
- d. Pedestrian facilities shall not be required should they prove detrimental to wetlands, floodplains, streams and/or historic/social resources, as determined by the City Engineer.

- e. Approval for exceptions must be granted by the Director of Community Services in consultation with the City Engineer and the Capital Projects and Community Services Committee.

In addition, sidewalks shall be integrated with facilities for other modes of transportation including bicycles, motor vehicles, buses and trains. Sidewalk improvements shall also take into consideration associated costs, environmental impacts and handicap accessibility. The City's *Complete Streets Policy* includes the following provisions:

- a. Provide pedestrian connections to walking trip generators such as employment, education, residential, recreational and public facilities and retail and transit centers.
- b. Utilize Complete Streets checklist for Planning and Zoning Board applications.
- c. Long-term transportation facilities should have pedestrian facilities.
- d. Install pedestrian facilities at corridor crossings, intersections, interchanges and bridges.
- e. Pedestrian facilities shall be designed and constructed in accordance with the best currently available standards and practices, including the New Jersey Roadway Design Manual, the AASHTO Guide for the Planning, Design and Operation of Pedestrian Facilities and the Manual of Uniform Traffic Control Devices, among others.
- f. Provision should be made for pedestrians when closing roads, bridges or sidewalks for construction projects as outlined in NJDOT Policy #705 – Accommodating Pedestrian and Bicycle Traffic During Construction.
- g. Improvements shall consider Safe Routes to Schools, Safe Routes to Transit, Transit Villages, trail crossings and areas or populations with limited transportation options.
- h. Improvements shall comply with Title VI Environmental Justice, Americans with Disabilities Act (ADA) and complement the context of the surrounding community.

3. New Jersey Department of Transportation Complete Streets Policy:

Section VII states "Improvements should consider connections for Safe Routes to Schools, Safe Routes to Transit, Transit Villages, trail crossings and areas of population groups with limited transportation options."

4. New Jersey Department of Transportation Complete Streets Guide:

Section six states, "Complete and well-maintained sidewalk networks, accessible transit stops, properly placed and designed curb ramps and other accessible features make it easier for all people to travel and provide a more dignified and aesthetically pleasing built environment."

Page 4 states, "Sidewalks promote health and safety according to Social Science & Medicine Journal which found that residents are 65 percent more likely to walk in a neighborhood with sidewalks."

Page 5 states, "Sidewalks also lead to increased traffic safety. The FHWA report entitled *A Review of Pedestrian Safety Research in the United States and Abroad* demonstrates "that pedestrian crashes are more than twice as likely to occur in places without sidewalks, while locations with sidewalks on both sides of the road have the fewest crashes."

Page 33 states, "Finally, sidewalks are an extension of the street system. They are the primary conduit for pedestrian travel and fundamental to facilitating residential, commercial and social activity in urban, suburban and rural communities. Sidewalks provide access between buildings and provide space for dynamic street life. Sidewalks, particularly in commercial and downtown areas, form the foundation for a vibrant community. They should be safe, comfortable and attractive facilities that provide accommodations for people of all ages and abilities."



EXISTING CONDITIONS

1. Sidewalks

The City of Summit consists of over 80 miles of paved roads and approximately 117 miles of sidewalks. The existing sidewalks are predominantly located in the center of town and extend outward towards the City limits. The majority of the sidewalks are concrete, although some are composed of gravel, stone, slate, brick and asphalt. The Engineering Division has completed an inventory of the City's entire sidewalk network which is included in the appendix of this report.

2. School Walking Routes

The City of Summit consists of two (2) primary centers, five (5) local elementary schools, one (1) middle school and one (1) high school that serve the residents, with several private schools as well. The elementary schools each have their own designated walking routes created and maintained by each school's Parent-Teacher Organization (PTO) in conjunction with the Summit Police Department, Board of Education and the City's Engineering Division. The walking routes are included in the appendix of this report.

3. School Walking Route Data

Student mode of travel tallies were taken over three (3) days at each school in November 2017. A questionnaire was provided to the teachers at each respective school. The responses and data were then forwarded to TransOptions, one of eight Transportation Management Associations (TMAs) in New Jersey, and compiled into reports which were then returned to the City. TMAs are nonprofit organizations that implement strategies and programs addressing traffic congestion, economic development, air quality and livability issues.

Each school was questioned regarding the number of trips to/ from school, both in the mornings and afternoons, and whether students walk, bike, bus, carpool, drive or use some other means of transportation to and from school. That data was then compiled into percentages of the school student population. The summary data from each school for the three (3) days is outlined below:

Brayton Elementary School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	1047	42%	1%	0.5%	50%	6%	0.6%	0.5%
Afternoon	998	41%	1%	1%	49%	7%	0.6%	0%

Jefferson Elementary School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	687	8%	0.1%	2%	84%	5%	0%	0.1%
Afternoon	687	22%	0.1%	3%	69%	6%	0%	0.7%

Washington School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	924	22%	1%	1.0%	68%	7%	0%	0.5%
Afternoon	939	30%	1%	5%	56%	7%	0.0%	0.5%

Summit High School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	2451	13%	2%	0.3%	64%	19%	0.4%	1%
Afternoon	2053	25%	2%	0.4%	51%	21%	0.3%	1%

Lawton C. Johnson Middle School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	2587	20%	2%	0.2%	64%	12%	0.4%	0.2%
Afternoon	2567	42%	2%	0.1%	46%	9%	0.2%	0.3%

Lincoln-Hubbard School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	859	30%	0.3%	0.8%	62%	7%	0%	0.1%
Afternoon	821	34%	0.5%	3%	54%	7%	0%	0%

Franklin School

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	995	21%	0.4%	2%	67%	9%	0%	0.3%
Afternoon	975	28%	0.3%	2%	58%	11%	0%	0.3%

Jefferson Primary Center

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	265	5%	0%	2%	89%	5%	0%	0%
Afternoon	216	8%	0%	7%	76%	8%	0%	0%

Wilson Primary Center

	Trips	Walk	Bike	Bus	Vehicle	Carpool	Transit	Other
Morning	348	4%	0.3%	2%	91%	2%	0%	0%
Afternoon	331	15%	0%	0.6%	79%	5%	0%	0%

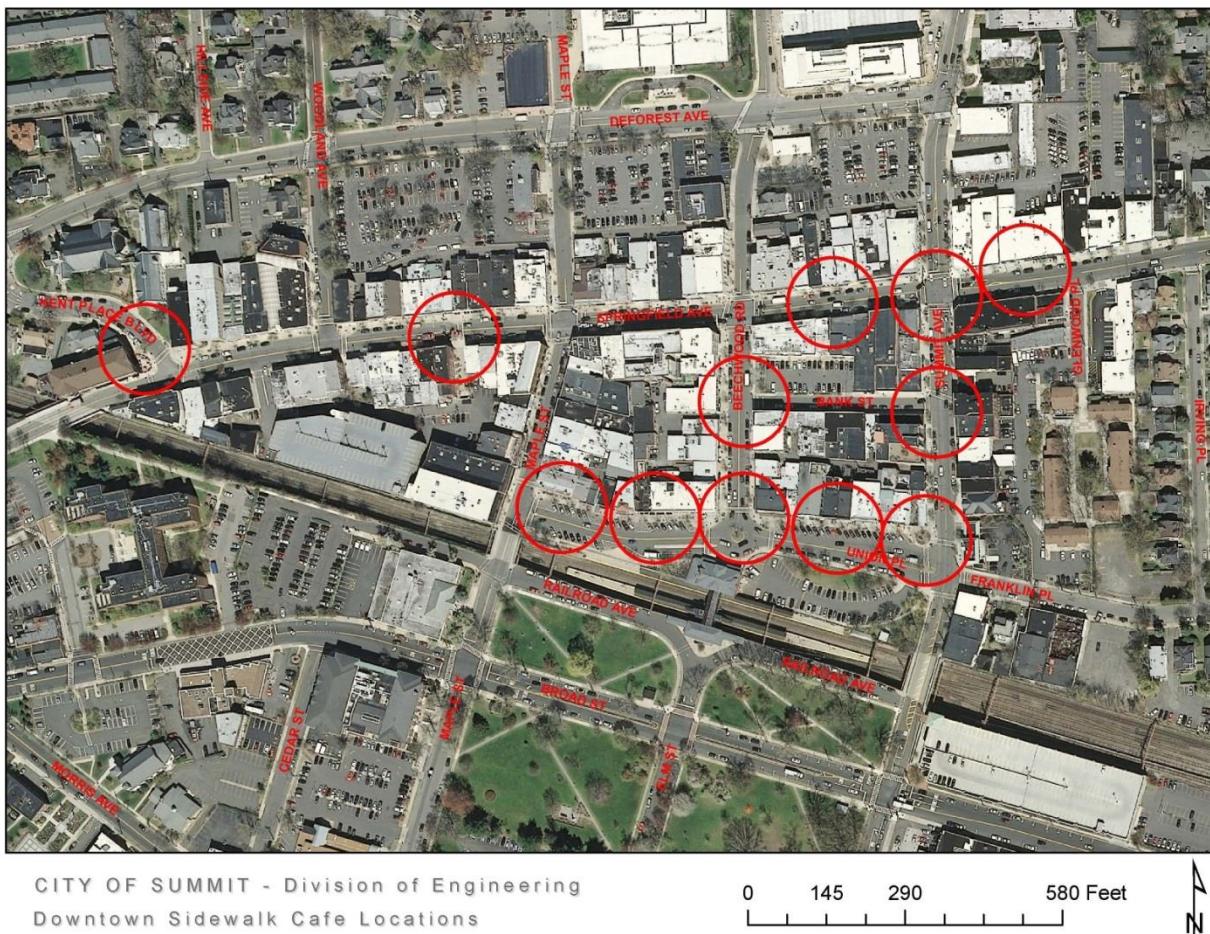
Based on the data collected, the average number of students walking to school on a typical day based on the respondents and data above is approximately 1,693 children, not including any parents, siblings, family members or other person walking with the student to and from school.

The Primary Centers will always generate more vehicular traffic as opposed to pedestrian traffic due to the age of the attending students and their possible inability to walk long distances over a short period of time.

The data above demonstrates that on average over a quarter of the City of Summit public school student population is walking to or from school.

4. Sidewalk/Outdoor Dining

Existing seating and dining facilities for sidewalk cafes are currently allowed within the CRBD. Sidewalk dining is permitted in accordance with Chapter 4, Section 20 of the City's Revised General Ordinance (RGO), known as "Regulating of Sidewalk Cafes" limits such facilities to the Central Retail Business District (CRBD). Section 35-5.3 of the City's Development Regulations Ordinance (DRO) also contains specific provisions for the design of sidewalk cafes. Where provided, sidewalk cafes are to maintain a path of at least four (4) feet in width for pedestrian traffic. Canopies, umbrellas, landscaping and fencing are typical amenities permitted by ordinance. The locations of the outdoor dining facilities may vary depending on the location of the business currently in operation. However, historically, the same locations are occupied year after year. These sidewalk cafes must be continually maintained by the restaurant owners to afford the proper clearance for sidewalks. Below is a map depicting the approximate locations of the continually operated restaurants with a sidewalk café along with a typical sidewalk café located within the downtown.



Map created by the City of Summit Division of Engineering



Intersection of Kent Place Boulevard and Springfield Avenue

RECOMENDATIONS

1. Consistency among various plans

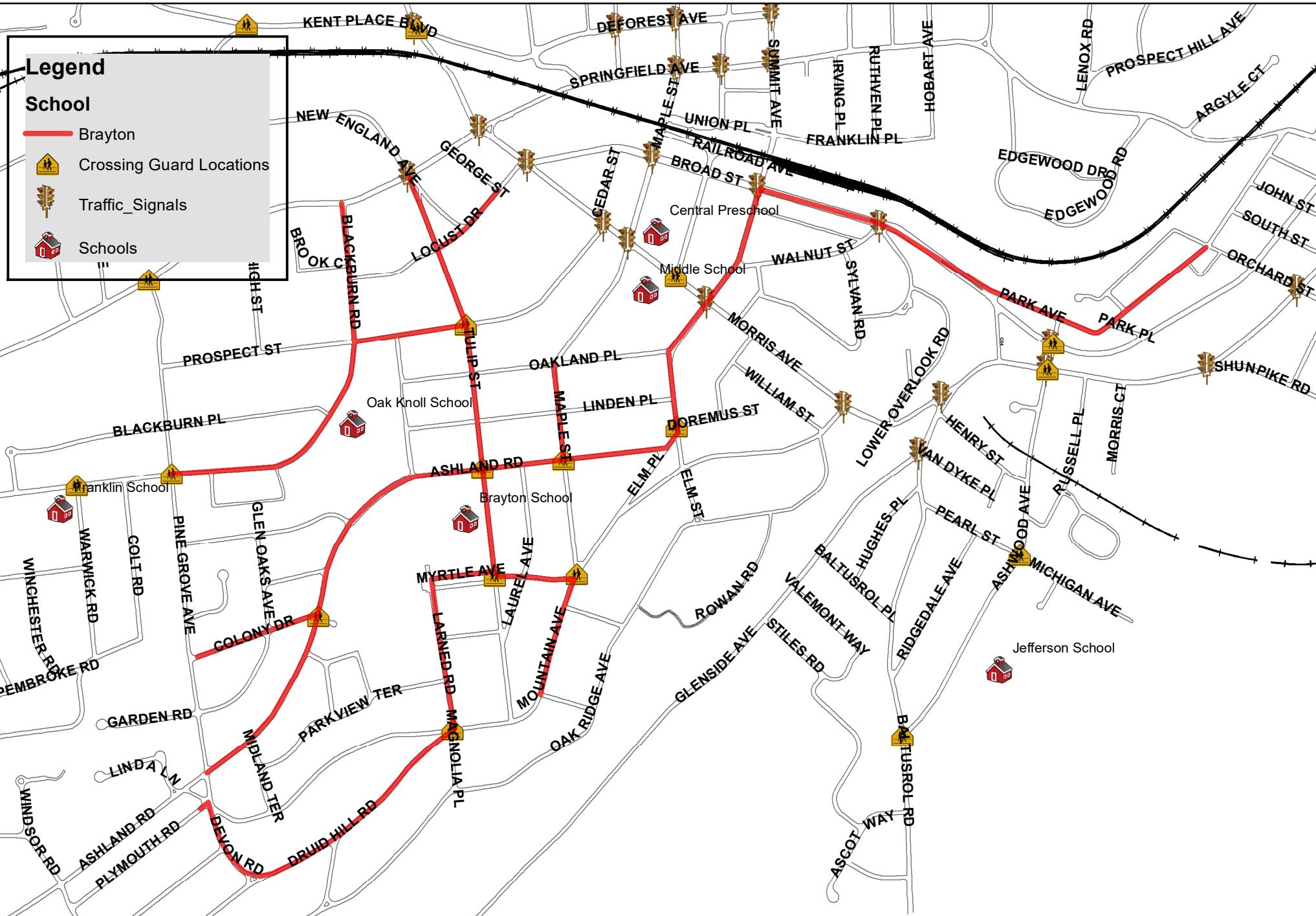
It is the intent of this *Sidewalk Installation and Maintenance Plan* (Plan) to remain consistent with the City's other development policies, plans and ordinances and to promote consistency amongst all of these other documents. Therefore, the Plan should be applied in concert with the City's existing development regulations and recommends a comprehensive review of all ordinances, policies, and practices currently in place. Once this plan is adopted, a review and any subsequent amendments and or revisions shall be recommended within six (6) months.

2. Assessments

Currently, the cost for new and replacement sidewalks is assessed to the adjacent property owner. The City should complete a full evaluation of this practice and consider abandoning it within three months of approval of this Plan. Cost has been a common argument for the affected property owners who are opposed to the installation of sidewalks. The maintenance and repair of sidewalks is currently the responsibility of the property owner and this is common practice statewide. No change to this practice should be considered by the City at this time.

3. School Walking Routes

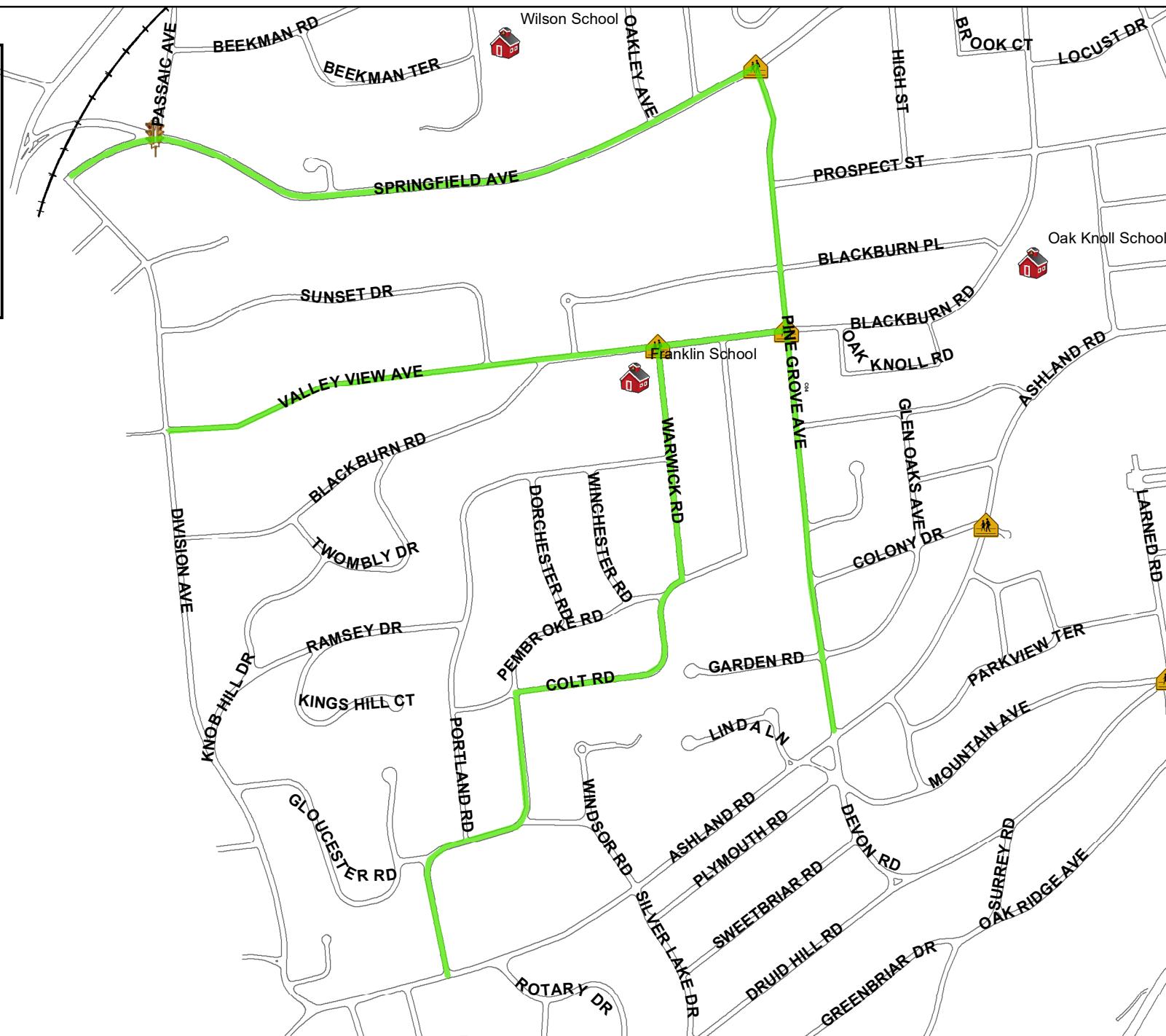
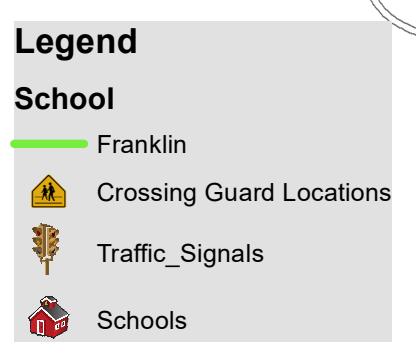
Each elementary school, in conjunction with their respective PTO, school staff, Department of Community Services and the City's Police Department should review and update the designated walking routes every two (2) years including an initial review by August 2020. The descriptions of the school walking routes, provided in the appendix should also be updated to reflect the current routes show below. Input should be sought from parents, students, teachers and City staff for future changes. The walking routes should be endorsed by the Summit Board of Education, the Public Safety Committee and the City of Summit Common Council. The process of updating the current designated safe routes to school began with the preparation of this document and revised routes are included below for each of the 5 elementary schools.



CITY OF SUMMIT - Division of Engineering
BRAYTON SCHOOL WALKING ROUTE
Sept 2019

0 700 1,400 2,800 Feet

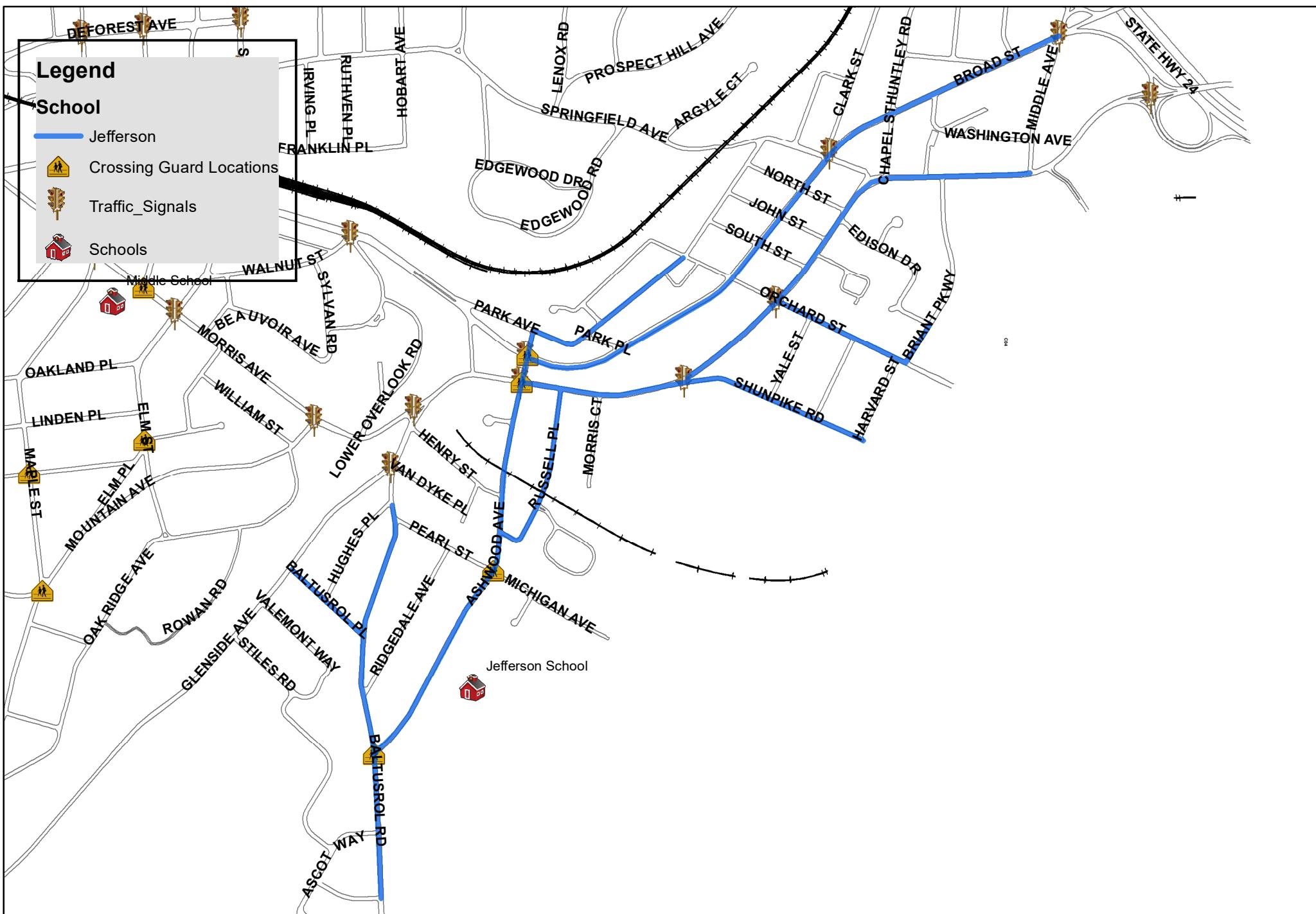




CITY OF SUMMIT - Division of Engineering
FRANKLIN SCHOOL WALKING ROUTE
Sept 2019

0 500 1,000 2,000 Feet

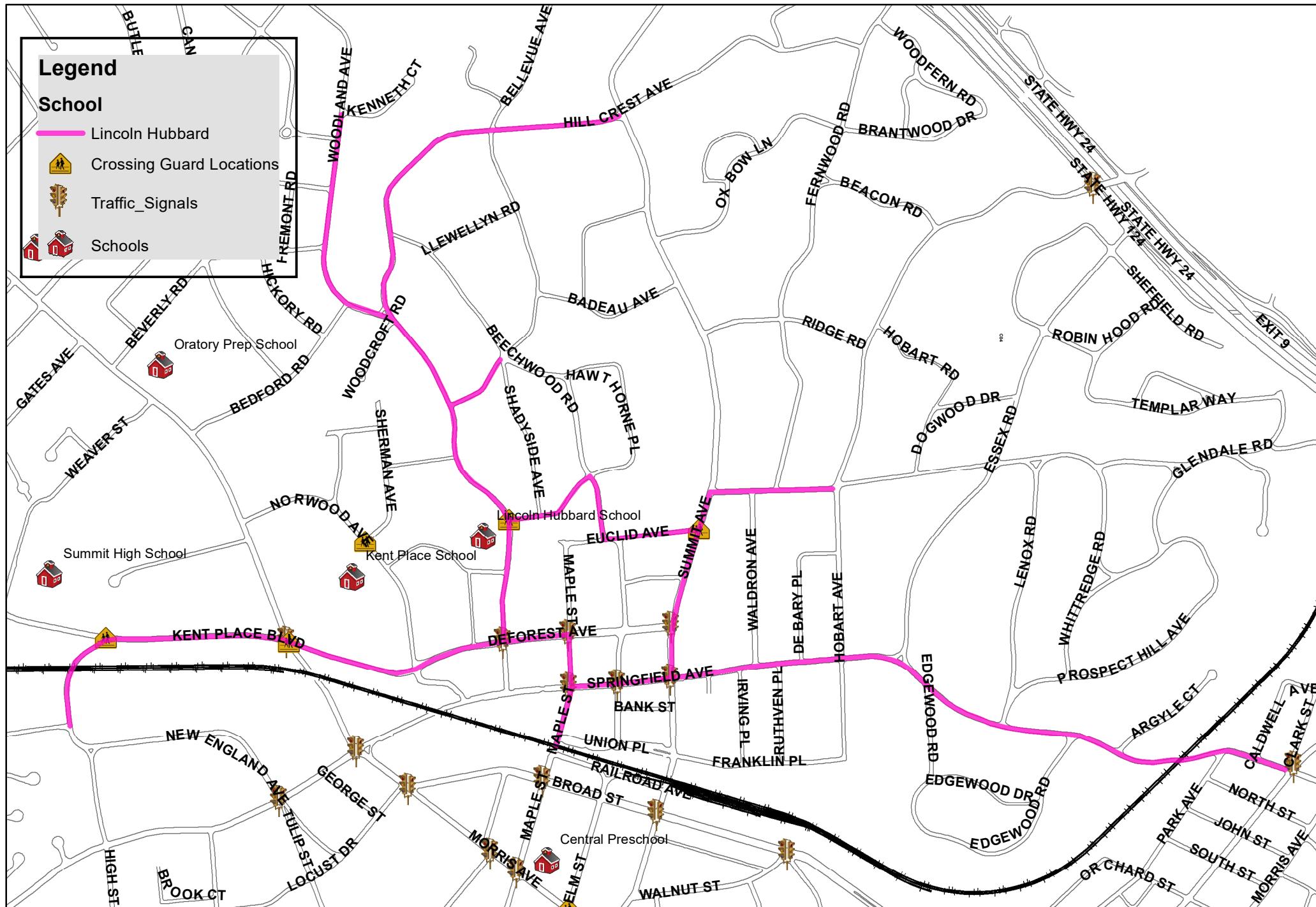
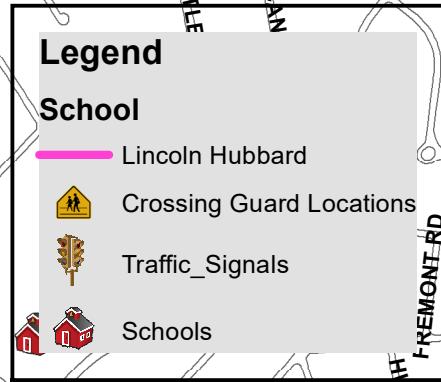




CITY OF SUMMIT - Division of Engineering
JEFFERSON SCHOOL WALKING ROUTE
Sept 2019

0 625 1,250 2,500 Feet





CITY OF SUMMIT - Division of Engineering
LINCOLN HUBBARD SCHOOL WALKING ROUTE
Sept 2019

0 550 1,100 2,200 Feet



Legend

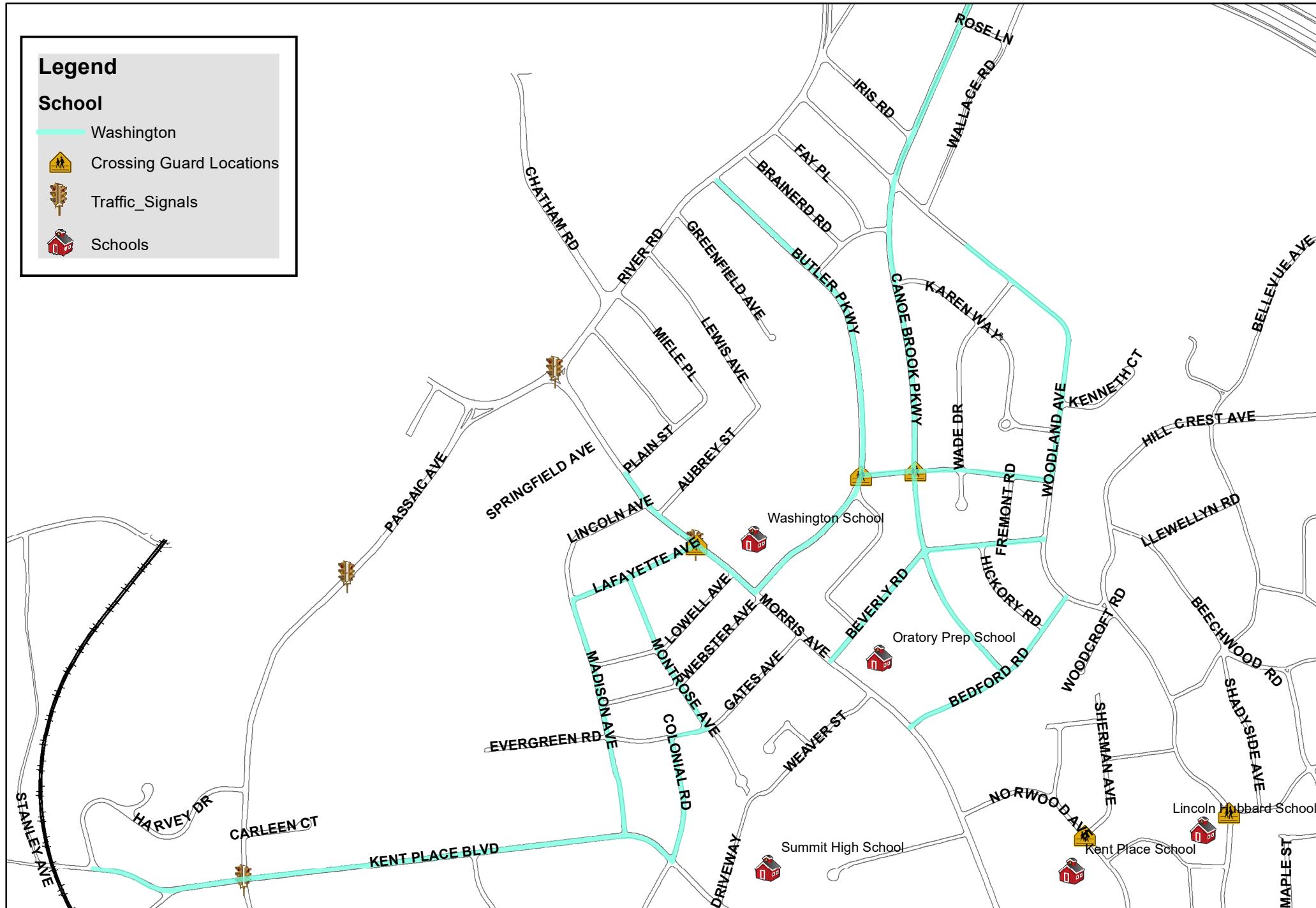
School

Washington

Crossing Guard Locations

Traffic_Signals

Schools



CITY OF SUMMIT - Division of Engineering

WASHINGTON SCHOOL WALKING ROUTE

Sept 2019

0 550 1,100 2,200 Feet



4. Sidewalks

a. New Sidewalks

One of the main goals of this plan is to identify critical areas where sidewalks are needed based on the current policy. Below this section, a map is included depicting the existing and proposed sidewalk locations throughout the City. These proposed locations include sidewalks that would complete large gaps, eliminate smaller missing links, and or are located along school walking routes. It is important to note that the current policy recommends sidewalk on at least one side of the street along school walking routes. Locations that warrant installation on both sides are shown (i.e. Broad Street). For areas where the optimum side cannot be identified without formal engineered plans (i.e. Pine Grove), they are shown on both sides. In addition, this plan includes the following additional locations:

Street	From/To	Side
New Providence Avenue	Passaic Avenue to New Providence Avenue	East Side
Hobart Avenue	Whittredge Road to Arboretum	East Side
Woodland Avenue	Bedford Road to Canoe Brook Parkway	West Side
Glenside Avenue	Glenside Field to Baltusrol Road	South Side

These locations are proposed based on requests from neighborhood, potential pedestrian traffic generating points, speed limits of adjacent roads and need to increase pedestrian safety.

b. Upgrade of Existing Sidewalks

There are several City locations that have sidewalks which do not foster a sense of safety for pedestrians walking along those routes. These sidewalks should be upgraded and improved via widening, grading changes and other pedestrian safety improvements such as crosswalks, pedestrian signs, and landscape improvements. Some of the locations that would benefit from improved sidewalks are:

- Broad Street – Route 24 Bridge to Broad Street Intersection, South Side, increase the width of sidewalk to accommodate more pedestrians accessing the business district on the other side of Route 24, increase safety and comfort level of pedestrian along a busy street.

- River Road – Entire length, North and South sides, increase width of sidewalk to accommodate more pedestrians and improve walkability and install additional sidewalk.
- Morris Avenue – Entire length, East and West sides, increase width of sidewalk, add additional signs and increase visibility.
- All Walking Routes – All walking routes should be considered for upgraded sidewalks to a minimum width of 5' particularly in areas of high pedestrian volume.

c. Other Sidewalks throughout the City

The City shall continue to install sidewalks in appropriate locations that are not within the required school walking routes or other designated areas. Residents of neighborhoods or particular streets without sidewalks may petition the City for sidewalks. In these instances, signed petitions of at least 66% (2/3) of the residents located on the sidewalk receiving side of the street must be attained. The proposed new sidewalk installation project would then be considered for the next pedestrian safety improvement project schedule. Such sidewalk installations shall be funded by the City in their entirety and maintained moving forward, after installation, by the adjacent property owner.

Below are maps depicting the City-wide sidewalk network with the potential addition of sidewalks as described above and then maps highlighting each school walking route with the existing and proposed sidewalks:

LEGEND



CITY OF SUMMIT
CITYWIDE
SIDEWALK INSTALLATION & MAINTENANCE POLICY
EXISTING SIDEWALK WITHIN CITY LIMITS

CITY OF UNION CO., N.J.
DIVISION OF COMMUNITY SERVICES
512 SPRINGFIELD AVENUE
SUMMIT, NJ 07901
UNION COUNTY

Aaron J. Schrager
Professional Engineer
New Jersey Lic. No. 46143
City Engineer



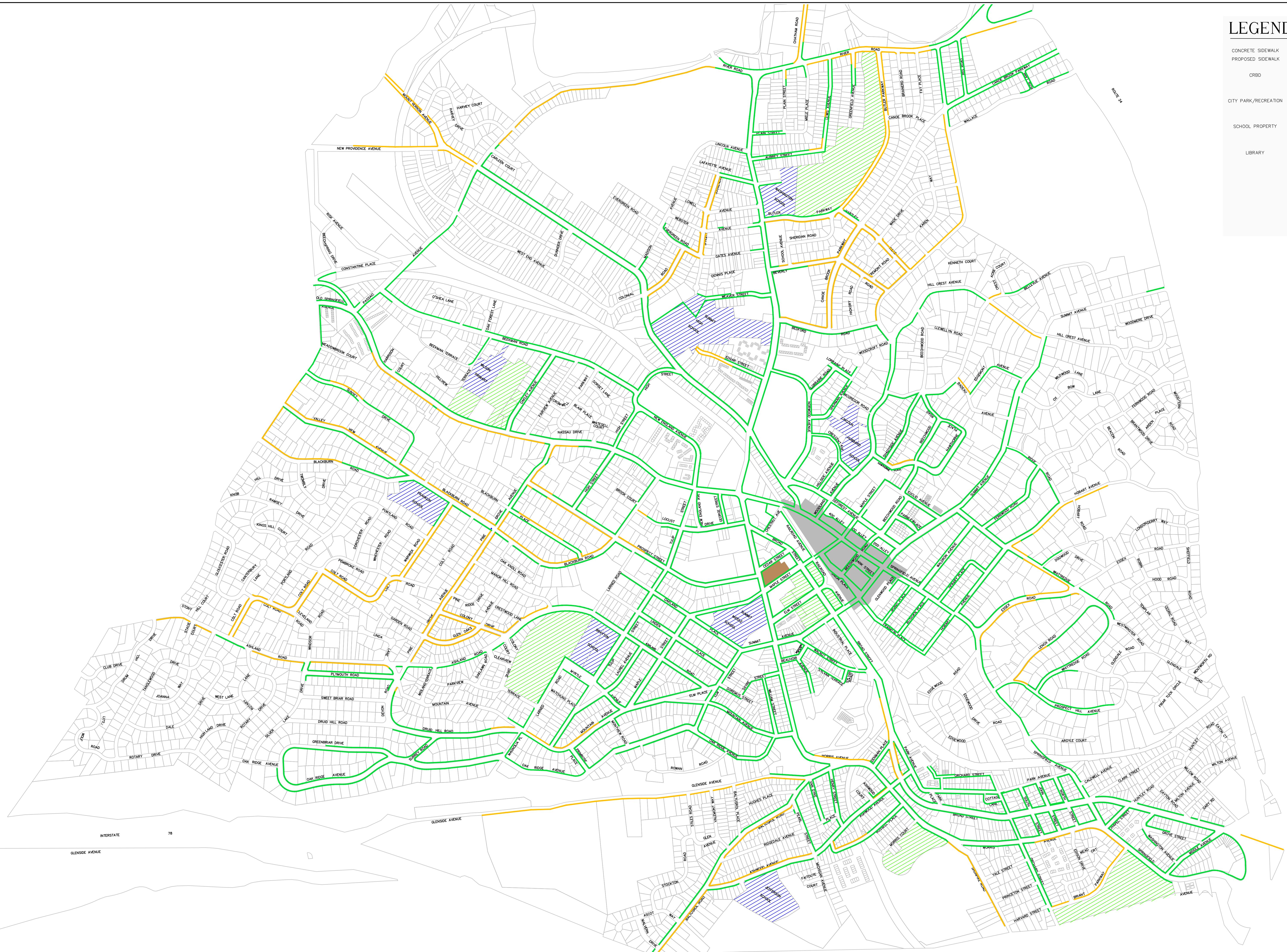
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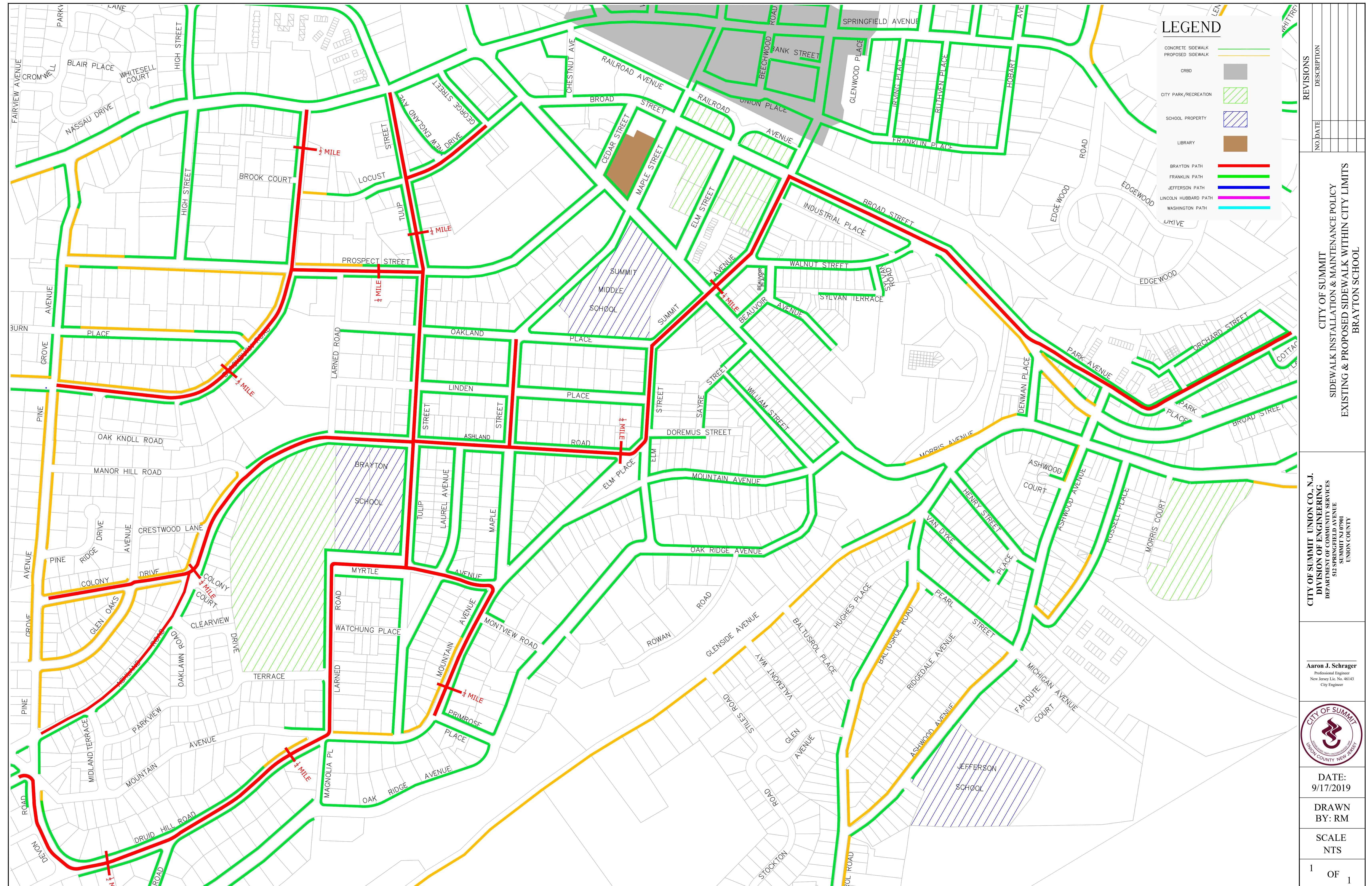
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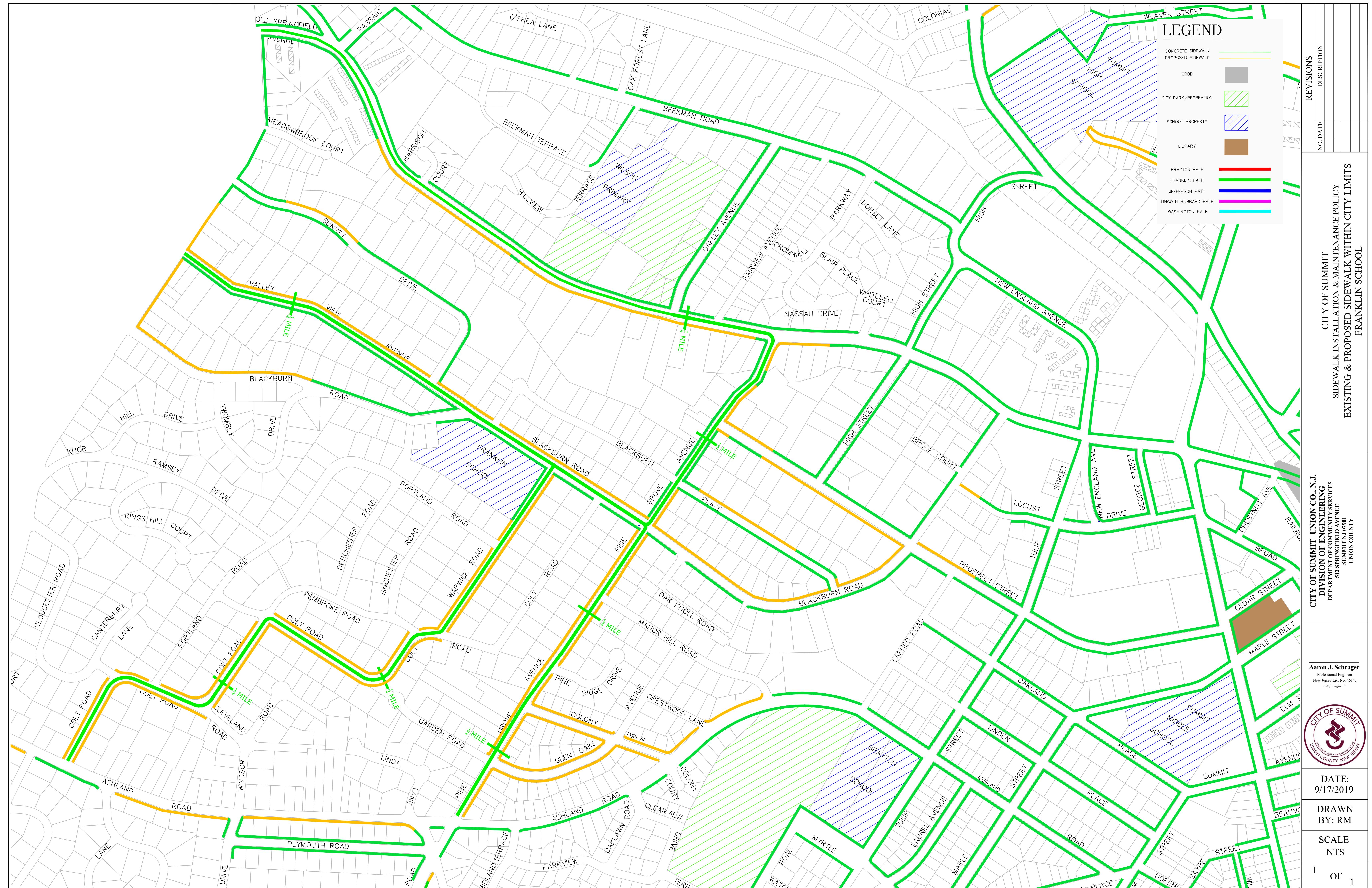
SCALE
NTS

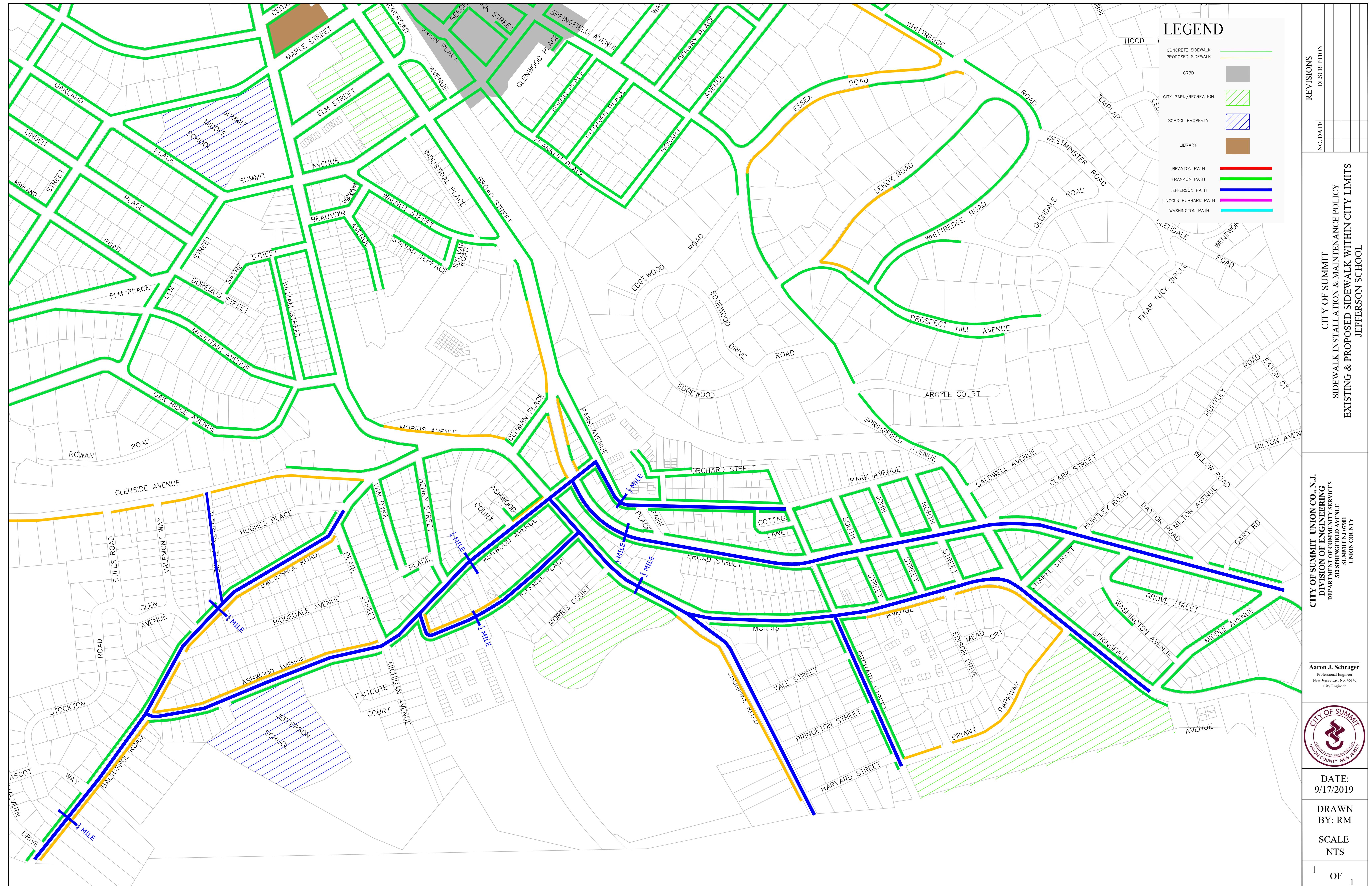
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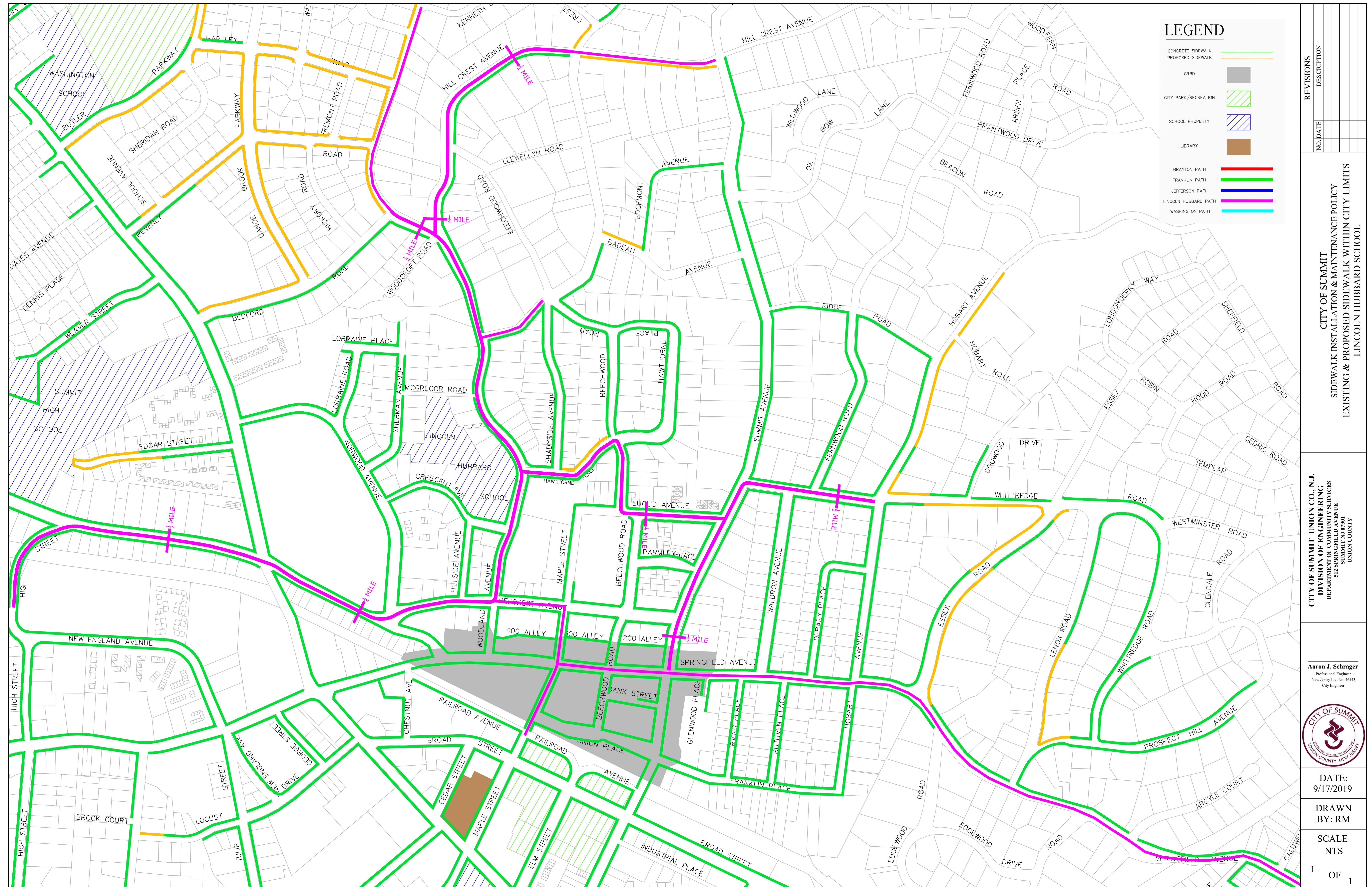


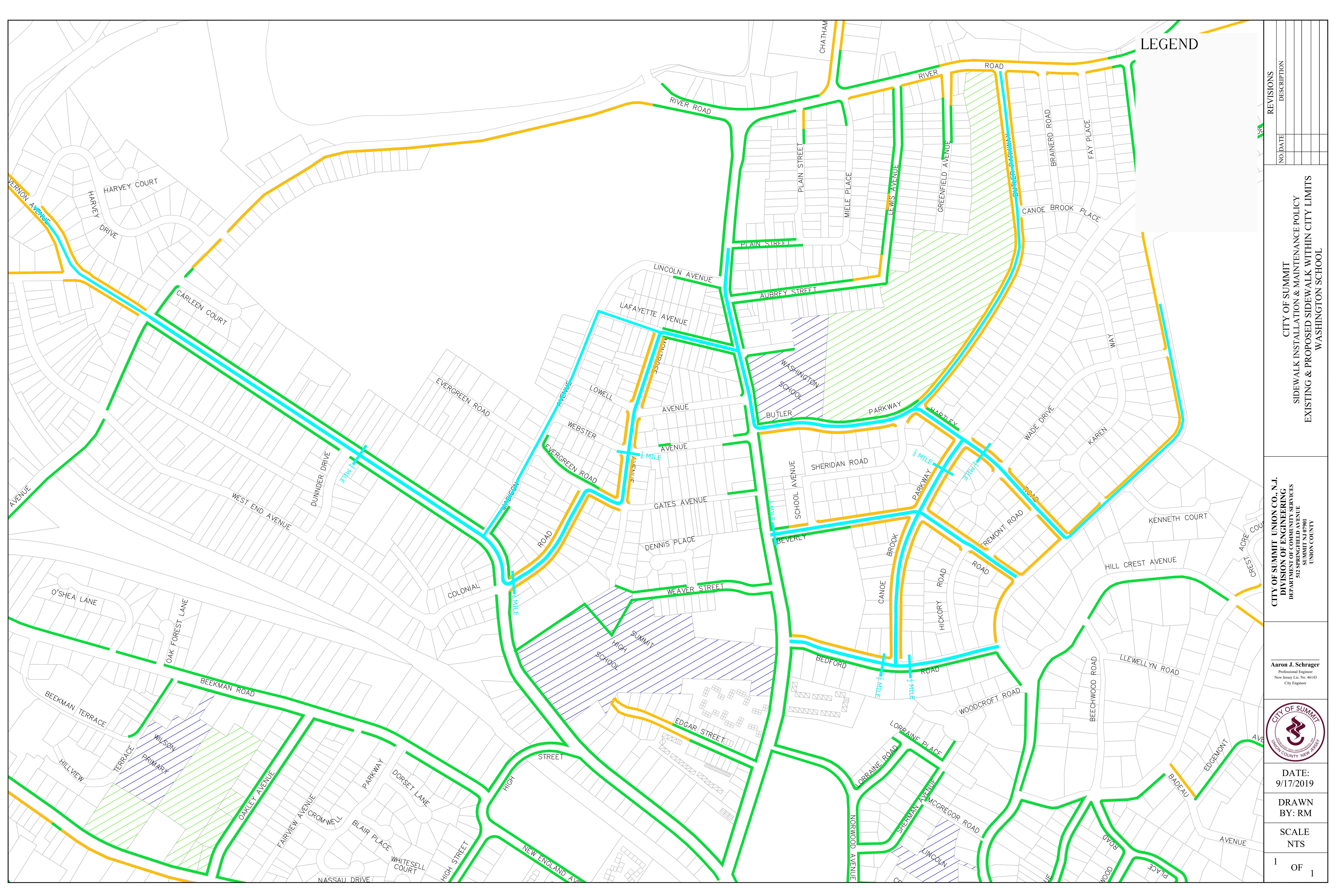












5. Sustainability

The City recognizes the potential environmental impacts of sidewalks such as increased impervious coverage and runoff, increase in the potential of heat island effects and resident maintenance costs. These impacts must be weighed against the need to provide for pedestrian safety while encouraging walking as a means of transportation, promoting a sense of community and reducing carbon emissions. With regard to environmental impacts, the following shall be considered when installing or repairing sidewalks:

Tree well photo taken on Maple Street

- a. Minimize the potential damage to, or removal of any mature trees, including landmark or heritage trees. Other types of sidewalks may be considered to minimize the impact to adjacent trees such as asphalt sidewalks and rubber sidewalks.
- b. Minimize impacts to wetlands, ponds, streams, rivers or other areas designated under the City's Environmental Resource Inventory as a sensitive area, including limiting surface runoff where feasible.
- c. Utilize environmentally friendly products, processes and equipment in the construction and maintenance of sidewalks to the extent possible.
- d. Install plant material near or adjacent to sidewalks to promote an aesthetically pleasing environment which can be reasonably well maintained and will not pose safety issues to pedestrians, bicyclists or motorists.

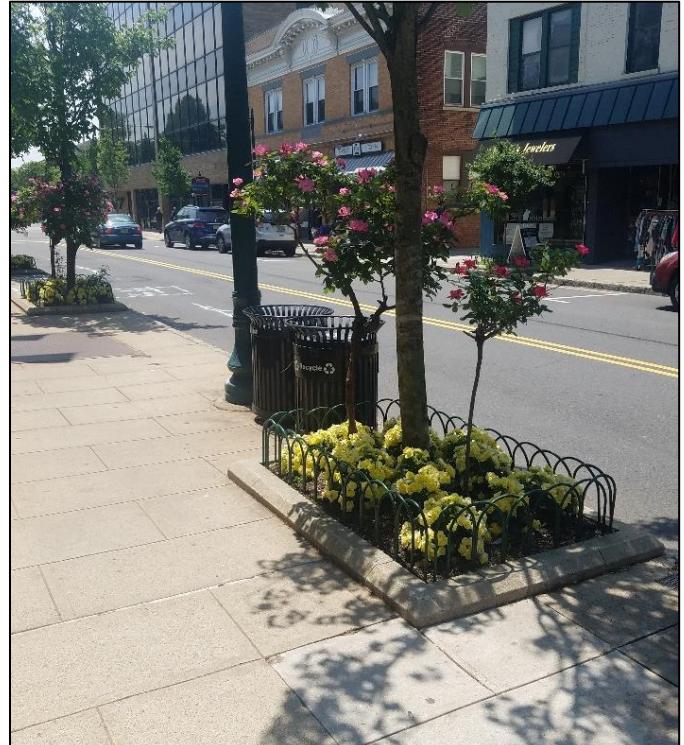


Photo taken from the corner of Bank Street and Beechwood Road

- e. Evaluate the feasibility of adding street amenities to encourage walking such as art, plantings, seating, lighting, etc.
- f. Where possible, offset the increase of impervious coverage added from new sidewalk within the City by reducing unused road areas, unused parking areas and other unused impervious surface areas.
- g. Plant a tree of an appropriate species under advisement of the City Forester for every 50 linear feet of sidewalk installed either adjacent to the area of somewhere within City limits.
- h. Use permeable pavement where applicable and appropriate.



6. Street Crossings

- a. All street crossings must have adequate safety measures installed including, but not limited to, a striped crosswalk, signage, signalization, lighting and/or a traffic calming measure as determined to be appropriate by an engineering study. A study shall consider location, visibility, topography and vehicular/pedestrian volumes.
- b. The use of a high friction surface treatment is encouraged on crosswalks adjacent to schools, parks and other City amenities.
- c. Landscaping that is encouraged along sidewalks should be kept to a 24" minimum height within 150 linear feet from a crosswalk.



Photo taken looking at the intersection of Maple Street and Springfield Avenue

- d. All signalized pedestrian crosswalks should be inspected by the City regularly for performance, operability and safe access.
- e. The City's commitment to pedestrian safety funding should continue.



Photo taken from Ashwood Avenue looking towards Jefferson School

- f. Pedestrian crosswalk distances should be minimized, where possible, when a capital improvement project is underway via curb extensions, crossing islands and road diets (reduction of road asphalt with the addition of greenspace).

Photo taken at the corner of Woodland Avenue
and Springdale Avenue

- g. All mid-block crossings must have the proper signs installed at and ahead of the crosswalk per MUTCD standards.
- h. An RRFB device may be installed at a street crossing where deemed necessary by an engineering study. A study shall consider location, visibility, topography and vehicular/pedestrian volumes.
- i. Where applicable, County and/or State approval must be sought prior to installing any devices or measures on or adjacent to Union County and NJ DOT roads.



7. Sidewalk Amenities

- a. Where applicable, sidewalks and pedestrian walkways may be supplemented with appropriate plantings, lighting, street furniture, recycling receptacles and public art.
- b. Amenities are to be installed to enhance the City's walkability and pedestrian experience while promoting safety, interest, culture, healthy living and education.
- c. Features should be placed in appropriate locations to minimize negative impacts to sidewalk integrity, and to reduce maintenance and safety concerns. Temporary installations for special events, holidays and/or seasons are encouraged.



Photo taken from Springfield Avenue looking towards Kaus Way

- d. Holiday lighting within the downtown is also highly encouraged as it illuminates pedestrian walking areas during the darkest time of the year and increases the interest and culture of the walking route.

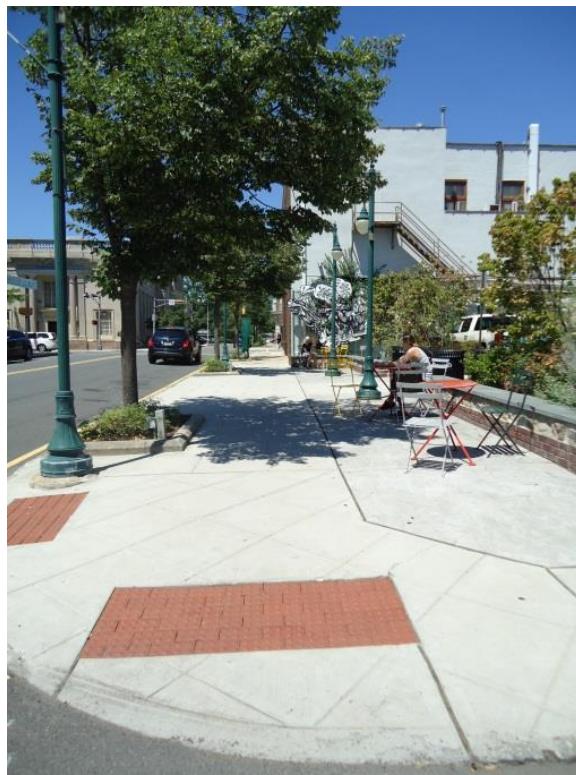


Photo taken from Bank Street Looking North towards Lyric Park

- e. Special seating areas including seating walls, benches, stools, patio furniture, etc. similar to the Lyric Park shown above, are also encouraged.
- f. Dedicated sidewalks are encouraged for historical and memorial purposes but will require Common Council approval upon recommendation of the Public Safety Committee.

8. Prioritization

Priority shall not be given to any one single section of proposed sidewalk. However, various criteria should be considered when determining when and where to install new sidewalks. In no specific order, sidewalk prioritization shall include, but not be limited to:

- *Current available funding:* If the City has available funds for a specific pedestrian improvement, they should be used for such immediately.
- *Impending capital improvement project:* If the City is currently preparing or has prepared a capital improvement project that may include any sidewalk within its limits, the project should progress as scheduled ensuring that the guidelines contained in this plan are adhered to.
- *School walking route:* If a location is on or along a school designated walking route and sidewalks or sidewalk upgrades are needed, the City should proceed with setting aside capital funding followed by a capital project administered by the City Engineer.
- *Grant opportunities:* Grant opportunities shall be pursued for projects deemed credible via Safe Routes to School, Safe Routes to Transit, NJDOT Transit Village and NJDOT Local Aid.
- *Access to CRBD, NB:* Zones: Sidewalk links that connect residents, neighborhoods and high volume pedestrian areas to the CRBD and neighborhood businesses should be considered and follow processes listed above as applicable. Sidewalk should be installed on all streets within $\frac{1}{4}$ miles of the CRBD and NB zones. Streets without sidewalks shall be considered by the City Engineer and City Council for sidewalk installation during Capital Improvement projects and budget planning.



Photo taken of the intersection at Union Place and Summit Avenue

- *Access to other local or City amenities:* Access to any local or City amenity deemed necessary by the City Engineer shall also be considered for sidewalk installation or upgrade. Sidewalks may be installed on all streets adjacent to and around these amenities such as, public transit stops, parks, recreation facilities, public library, aquatic centers, parking and any other public facilities. The appropriate distances and locations of sidewalks relative to such facilities shall be at the discretion of the City Engineer.

Collectively, all of the above aspects may guide the decision making for the addition or upgrade of sidewalk facilities. These policy decisions shall be made by the City Council, with recommendations by City staff and Planning/Zoning Board where applicable.

MONITORING AND MEASUREMENT

1) Monitoring

An important aspect of the success of pedestrian improvement projects within the City will rely on monitoring changes to the pedestrian landscape by City staff, elected officials and residents.

Members of the public play a critical role in monitoring the pedestrian landscape since engineers, planners and staff are not available to be at each location where pedestrians may be traveling. It is imperative the public be involved and that they take an interest in the pedestrian paths they travel, and then share that information with the City staff. Contact information is available on the City's website.

2) *Measurement*

An essential part of the Plan is to have methods in place to measure success. A complete approach should be considered and may include counting the number of new or upgraded sidewalks, number of pedestrian related accidents, and pedestrian counts of usage on new or existing sidewalks. The City Engineer may conduct independent before and after studies for specific areas.

The City Common Council should determine the best method of measuring success via the use of the above methods with recommendations by the City Engineer. At the request of City Council, the City Engineer may, in collaboration with the City Police Department, prepare a report on a bi-annual basis to discuss the metrics of success of pedestrian improvement projects.

3) *Review*

A review of this Plan shall be conducted by the City of Summit every two (2) years. Any updates shall include a re-assessment of the existing conditions, a review of any and all policy documents approved or adopted since the previous release of this document and revisions to the recommendations section as needed. An evaluation of pedestrian accident reports prepared by the Summit Police Department shall also be used during the review process. The intent is for the City to remain proactive in addressing pedestrian safety issues.

The City's Engineering Division shall continue its regular evaluation and review along with the installation and maintenance of existing and proposed sidewalk improvements as part of all projects. Locations not specifically identified herein shall be considered, specifically those located in residential neighborhoods and major roadways with relatively high traffic volumes and speeds.

Additionally, Chapter 18 of the City's General Ordinance should be amended as necessary to maintain consistency with the *Summit Sidewalk Installation and Maintenance Plan*.

REFERENCES

- 1) New Jersey Department of Transportation – Complete Streets Design Guide
- 2) New Jersey Department of Transportation – Making Complete Streets a Reality
- 3) New Jersey Department of Transportation – School Zone Design Guide
- 4) City of Summit Master Plan Re-Vision 2016
- 5) Manual on Uniform Traffic Control
- 6) City of Summit Stop Sign Installation Guidelines